

Queensland



Subordinate Legislation 1995 No. 401

Transport Operations (Road Use Management) Act 1995

Transport Infrastructure (Roads) Act 1991

Traffic Act 1949

**TRANSPORT AND TRAFFIC AMENDMENT
REGULATION (No. 1) 1995**

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PART 1—PRELIMINARY

Short title

1. This regulation may be cited as the *Transport and Traffic Amendment Regulation (No. 1) 1995*.

PART 2—AMENDMENT OF TRANSPORT OPERATIONS (ROAD USE MANAGEMENT) REGULATION 1995

Regulation amended

2. This part amends the *Transport Operations (Road Use Management) Regulation 1995*.

Replacement of s 4 (Vehicles regulation applies to)

3. Section 4—

omit, insert—

‘Acts that are transport Acts

‘4.(1) Each of the following Acts is a transport Act—

- *Carriage of Dangerous Goods by Road Act 1984*
- *Motor Vehicles Control Act 1975*
- *Motor Vehicles Safety Act 1980*
- *State Transport Act 1960*
- *Transport Infrastructure (Roads) Act 1991*.

‘(2) Subsection (1) ceases to apply to an Act mentioned in it when the Act is repealed.

‘Approval of random inspection program

‘4A.(1) The chief executive may only approve a program for stopping private vehicles for the Act, section 47, if the program—

- (a) states that only an authorised officer who has successfully finished a particular course of training may exercise the powers of an authorised officer at a checkpoint; and
- (b) requires an authorised officer to wear a uniform approved by the chief executive when the authorised officer is on duty at the checkpoint; and
- (c) requires a police officer in uniform to be present at the checkpoint at night; and
- (d) establishes particular objective criteria for the random selection of motor vehicles for stopping at checkpoints.

Examples of objective criteria for random selection of motor vehicles—

1. Every fifth motor vehicle passing the checkpoint is to be checked.
2. Every second motor vehicle passing the checkpoint that appears to the authorised officer to be older than 5 years.

‘(2) The program may include other matters the chief executive considers appropriate.

‘(3) The exercise of a power under the Act, section 31(2)(a) is not invalid only because the authorised officer did not tell the driver the particular objective criterion used to select the driver’s vehicle for stopping the vehicle at a checkpoint.

‘(4) A training course mentioned in subsection (1) must educate authorised officers about the powers and duties of authorised officers under the Act and the appropriate way of exercising the powers and duties.

‘Regulation and control of powers of certain authorised officers

‘4B.(1) This section applies to an authorised officer, who is not a police officer, when exercising powers to stop a private vehicle.

‘(2) An authorised officer may only require the driver of a private vehicle to stop the vehicle during daylight.

‘(3) In addition, an authorised officer who is not wearing a uniform approved by the chief executive may only exercise the powers of an authorised officer in relation to a private vehicle that the authorised officer reasonably believes is so dangerous as to be likely to cause loss of life or bodily injury to a person.

‘Ways to indicate requirement to stop motor vehicle

‘4C. An authorised officer is to indicate in the following ways to the driver of a motor vehicle to stop—

- (a) by displaying a sign in the approved form;
- (b) by signalling in a way a police officer is required to signal under the *Traffic Regulation 1962*, section 21(2).’.

Amendment of s 5 (Fees)

4. Section 5—

insert—

‘(5) If a person registers a vehicle in 1 type in schedule 1, the person must not use the vehicle as a vehicle of another type to which a higher registration fee applies, unless it is registered under the other type.

Maximum penalty—80 penalty units.

Example of subsection (5)—

If a vehicle is not registered to haul a trailer, the vehicle cannot be used to haul a trailer unless its registration is changed and the appropriate registration fee is paid.’.

Insertion of new pts 3 and 4

5. After section 11—

insert—

‘PART 3—MASS

‘Application of part

‘12. This part applies to the following when on a road—

- (a) a vehicle with a GVM of more than 4.5 t;
- (b) a combination including a vehicle with a GVM of more than 4.5 t.

‘Performance standards for mass

‘13. The performance standard for mass is the mass (not more than the GVM or GCM) a vehicle and its load may achieve if the appropriate fees are paid.

‘Compliance with requirements—vehicles

‘14. The person in control of a vehicle must ensure the vehicle complies with a mass requirement applying to it.

Maximum penalty—50 penalty units.

‘Compliance with requirements—trailers

‘15. The person in control of a vehicle towing a trailer must ensure the trailer complies with a mass requirement applying to it.

Maximum penalty—50 penalty units.

‘Compliance with requirements—combinations

‘16. The person in control of a combination must ensure it complies with a mass requirement applying to it.

Maximum penalty—50 penalty units.

‘Vehicle tare

‘17. The tare of a vehicle may be determined—

- (a) by weighing the vehicle on a weighing device; or
- (b) from the certificate of registration in force for the vehicle under State or Commonwealth law.

‘Tyre mass

‘18. The tyre mass of a vehicle tyre may be determined by weighing on a weighing device.

‘Axle mass

‘19.(1) An axle mass of a vehicle may be determined—

- (a) for a single axle that is not part of a group of axles—by weighing with the tyres of the axle simultaneously on a weighing device; and
- (b) for a group of axles—by weighing with the tyres of the group of axles simultaneously on a weighing device.

‘(2) However, the axle mass of an axle fitted with dual tyres may be determined by placing only the outside tyres on the weighing device.

‘Vehicle mass

‘20. The mass of a vehicle or vehicle combination may be determined—

- (a) by weighing all axles simultaneously on a weighing device; or
- (b) by totalling the axle mass of each axle of the vehicle or vehicle combination.

‘Other ways of determining mass may be used

‘21. This part does not limit the way a vehicle’s tare or mass or a vehicle’s tyre or axle mass may be determined.

‘Livestock vehicles—use on roads

‘22. If a vehicle carrying livestock has a mass of more than a mass requirement applying to the vehicle, a person must not drive the vehicle on a road unless, when driving the vehicle, the person—

- (a) complies with the current guidelines; and
- (b) carries the guidelines in the vehicle.

Maximum penalty—80 penalty units.

‘Guidelines

‘23.(1) The chief executive may issue, in an approved form, guidelines about the movement of vehicles carrying livestock.

‘(2) The guidelines may be about—

- (a) roads on which the vehicle may or may not be driven; and
- (b) the maximum possible mass of the vehicle and its load; and
- (c) the type of vehicle that may be driven; and
- (d) rating requirements for vehicles under the Code of Practice—Commercial Motor Vehicle Modifications;¹ and
- (e) modifications that may need to be made to the vehicle; and
- (f) dimensions of the vehicle and stock crates; and
- (g) anything else relevant to the safe movement of the vehicle.

‘Transport of livestock

‘24.(1) This section applies to livestock vehicles registered in Queensland immediately before 1 July 1996.

‘(2) However, this section applies to a vehicle only if—

¹ The Code is published by the Australian Government Publishing Service and may be purchased from the Commonwealth Government Bookshop, Brisbane.

- (a) a permit under the *Transport Infrastructure (Roads) Regulation 1991*, section 3.05 applying to the vehicle was in force immediately before 1 July 1996; and
- (b) the vehicle does not comply with guidelines under this part.

‘(3) The owner of a vehicle to which this section applies may continue to use the vehicle to carry livestock.

‘(4) However, to continue to use a semitrailer to which this section applies, the owner of the vehicle must, before 1 July 1996, give to the chief executive a weighbridge certificate obtained after the commencement of this section and stating the vehicle’s tare.

‘(5) The owner must comply with permit conditions applying to the use of the vehicle immediately before the commencement of this section.

Maximum penalty—80 penalty units.

‘(6) For this section, the *Transport Infrastructure (Roads) Regulation 1991*, section 3.05 continues to apply to a vehicle to which this section applies as if it had not been repealed.²

‘(7) This section expires on 30 June 2003.

‘Phasing out of semitrailers used as livestock vehicles

‘25.(1) This section applies to a semitrailer with a tare of more than 15 t (a “**prescribed livestock vehicle**”).

‘(2) Section 24 ceases to apply to a prescribed livestock vehicle on the day specified for the vehicle in subsection (3) (the “**phase-out day**”).

‘(3) The phase-out day for a prescribed livestock vehicle is—

- (a) for a vehicle with a tare of more than 19 t—1 July 2001; or
- (b) for a vehicle with a tare of more than 17 t but not more than 19 t—1 July 2002; or

² To aid readers, the section is included in an attachment to this regulation. The attachment does not form part of the regulation.

- (c) for a vehicle with a tare of more than 15 t but not more than 17 t—30 June 2003.

‘(4) This section expires on 30 June 2003.

‘Information and liability offences

‘26. An offence against this part is—

- (a) an information offence for the Act, section 50; and
(b) a liability offence for the Act, section 57.

‘PART 4—LOADING

‘Application of part

‘27. This part applies to a load on a vehicle or combination.

‘Performance standards for loading

‘28. A way of loading a vehicle that ensures the following is a performance standard—

- (a) risks to other road users are minimised;
(b) the loading does not reduce the vehicle’s stability;
(c) the load is restrained to prevent it falling off the vehicle or dislodging when the vehicle is moving.

‘Compliance with requirements—vehicles

‘29. The person in control of a vehicle must ensure a load on it complies with a loading requirement applying to it.

Maximum penalty for a private vehicle—20 penalty units.

Maximum penalty for a heavy vehicle—35 penalty units.

‘Compliance with requirements—trailers

‘30. The person in control of a vehicle towing a trailer must ensure the trailer complies with a loading requirement applying to it.

Maximum penalty for a private vehicle—20 penalty units.

Maximum penalty for a heavy vehicle—35 penalty units.

‘Compliance with requirements—combinations

‘31. The person in control of a combination must ensure it complies with a loading requirement applying to it.

Maximum penalty for a private vehicle—20 penalty units.

Maximum penalty for a heavy vehicle—35 penalty units.

‘Proof of loading offences

‘32. In a proceeding for an offence against a loading requirement—

- (a) evidence that a load on a vehicle was not placed, secured or restrained in a way that met a performance standard under the Load Restraint Guide is evidence of a contravention of a loading requirement;³ and
- (b) evidence that a load, or part of a load, has fallen off a vehicle is evidence that the load was not properly secured, unless the contrary is proved; and
- (c) a court must presume a document purporting to be the Load Restraint Guide is the Load Restraint Guide, until the contrary is proved.

‘Information and liability offences

‘33. An offence against this part involving a heavy vehicle is—

³ The Load Restraint Guide is published by the Australian Government Publishing Service and may be purchased from the Commonwealth Government Bookshop, Brisbane.

- (a) an information offence for the Act, section 50; and
- (b) a liability offence for the Act, section 57.’.

Insertion of new schs 4 to 10

6. After schedule 3—

insert—

‘SCHEDULE 4

‘MASS REQUIREMENTS

schedule 11, definition “mass requirement”

‘Mass limits for tyres, wheels and axles

‘1.(1) The mass on a wheel or axle must not be more than the limit set by its manufacturer.

‘(2) The mass on a tyre must not be more than the highest load capacity determined for the tyre by the manufacturer at a cold inflation pressure of not more than—

- (a) for a radial ply tyre—825 kPa; or
- (b) for an other type of tyre—700 kPa.

‘(3) The mass on an axle group or single axle must not be more than the limit for the axle group or axle in schedule 5.

‘(4) For subsection (3), the mass limit for an axle group that includes a retractable axle must be determined as if the axle did not exist.

‘(5) However, a retractable axle is part of an axle group for schedule 5 if—

- (a) the mass on the group is more than—
 - (i) for a tandem axle group—6 t; or
 - (ii) for a triaxle group—11 t; and

- (b) the tyres on the axle are in contact with the ground and the load-sharing suspension system is operating on each axle, including the retractable axle, and each tyre in the group.

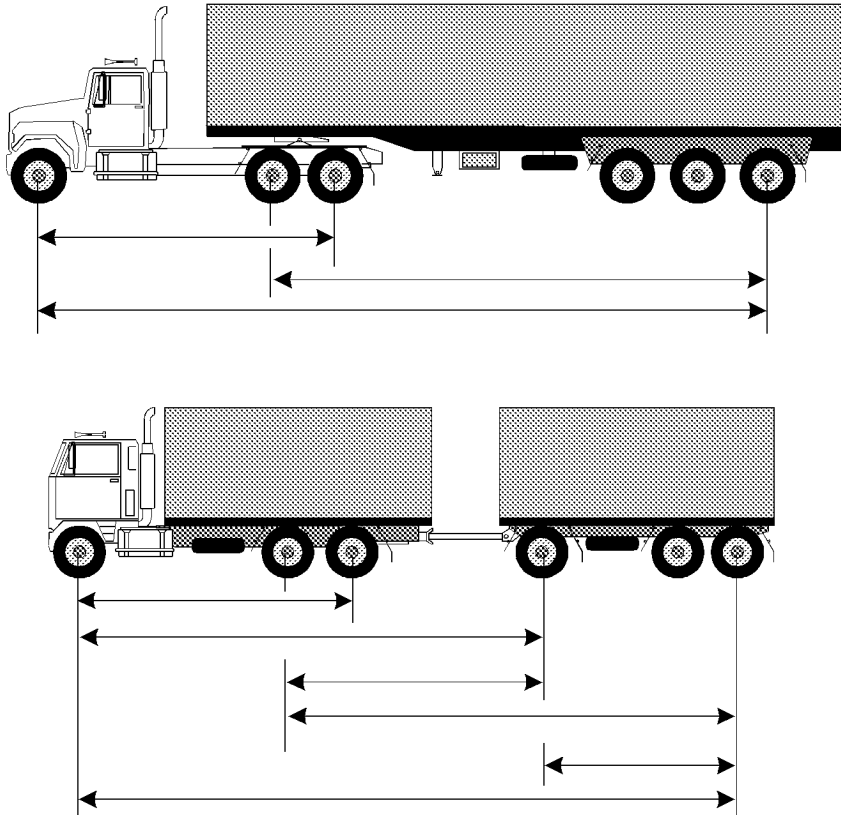
‘(6) The sum of the mass on the axle groups and single axles on a single vehicle or combination must not be more than the sum of the mass limits in schedule 5 for the axle groups and single axles.

‘Mass limits relating to axle spacing

‘2.(1) If the total mass of a vehicle or combination and a load on it cannot lawfully be more than 42.5 t, the mass limit for the vehicle or combination must not be more than the mass limit in the table in schedule 6 for the axle spacing distance for the vehicle or combination.

‘(2) Each distance is—

- (a) the distance from the centre of a single axle to the centre of another single axle; or
- (b) the distance from the centre of a single axle to the centre of the furthest axle in any axle group; or
- (c) the greatest distance between the centres of axles in any 2 axle groups.



Measurement of distances for schedule 6

‘(3) The mass limits apply to the sum of the mass on each axle group or single axle in the distance in the table, including the axles between which the distance is measured.

‘(4) If the distance between any 2 axles that are not part of the same axle group is less than 2.5 m, the total mass of a vehicle or combination and any load on it must not be more than 15 t.

‘Mass limit for a single vehicle

‘3. The total mass of a single vehicle and a load on it must not be more than the vehicle’s GVM.

‘Mass limits for combinations

‘4.(1) The total mass of a combination (other than a road train or B-double) and a load on it must not be more than 42.5 t.

‘(2) The loaded mass of a dog trailer or pig trailer must not be more than the loaded mass of the towing vehicle.

‘(3) The total axle mass of a tag trailer must not be more than the total axle mass of the towing vehicle.

‘(4) The total mass of a combination and a load on it must not be more than the towing vehicle’s GCM.

‘Load sharing suspension

‘5. The axles in each axle group (other than a twin steer axle group) must relate to each other through a load sharing suspension system.

‘Non-complying vehicles

‘6. If an axle or axle group on a vehicle or combination does not otherwise comply with this schedule, schedule 7 applies to the axle or axle group.

‘SCHEDULE 5**‘MASS LIMITS FOR SINGLE AXLES AND AXLE GROUPS**

schedule 4, section 1(3), (5) and (6)

| Description of single axle or axle group | Mass limit (tonnes) |
|---|--------------------------------|
| Single axles and single axle groups | |
| Single steer axle on a motor vehicle | 6.0 |

| | |
|--|------|
| Single axle or single axle group fitted with single tyres with section width of— | |
| (a) less than 375 mm | 6.0 |
| (b) at least 375 mm but less than 450 mm | 6.7 |
| (c) at least 450 mm | 7.0 |
| Single axle or single axle group fitted with dual tyres on— | |
| (a) a pig trailer | 8.5 |
| (b) a bus licensed to carry standing passengers | 10.0 |
| (c) another vehicle | 9.0 |
| Twinsteer axle groups | |
| Twinsteer axle group without a load-sharing suspension system | 10.0 |
| Twinsteer axle group with a load-sharing suspension system | 11.0 |
| Tandem axle groups | |
| Tandem axle group fitted with single tyres with section width of— | |
| (a) less than 375 mm | 11.0 |
| (b) at least 375 mm but less than 450 mm | 13.3 |
| (c) at least 450 mm | 14.0 |
| Tandem axle groups fitted with single tyres on 1 axle and dual tyres on the other axle | 13.0 |
| Tandem axle group fitted with dual tyres on— | |
| (a) a pig trailer | 15.0 |
| (b) another vehicle | 16.5 |
| Triaxle groups | |
| Triaxle group on a vehicle fitted with single tyres with section width of less than 375 mm on all axles, or single tyres on 1 or 2 axles and dual tyres on the other axle or axles | 15.0 |
| Triaxle group on a pig trailer with either single tyres with section width of at least 375 mm, dual tyres on all axles, or a combination of the tyres | 18.0 |
| Triaxle group, on a vehicle other than a pig trailer, with either single tyres with section width of at least 375 mm, dual tyres, or a combination of the tyres | 20.0 |

Quad-axle groups

| | |
|--|------|
| Quad-axle group fitted with single tyres with section width of less than 375 mm | 15.0 |
| Quad-axle group fitted with single tyres with section width of at least 375 mm or dual tyres | 20.0 |

SCHEDULE 6**MASS LIMITS RELATING TO AXLE SPACING**

schedule 4, section 2(1)

| Distance (metres) | | Mass limit (tonnes) |
|------------------------------|----------------------|--------------------------------|
| more than | not more than | |
| 0.0 | 3.7 | 23.0 |
| 3.7 | 3.8 | 23.5 |
| 3.8 | 4.0 | 24.0 |
| 4.0 | 4.2 | 24.5 |
| 4.2 | 4.3 | 25.0 |
| 4.3 | 4.5 | 25.5 |
| 4.5 | 4.7 | 26.0 |
| 4.7 | 4.8 | 26.5 |
| 4.8 | 5.0 | 27.0 |
| 5.0 | 5.2 | 27.5 |
| 5.2 | 5.3 | 28.0 |
| 5.3 | 5.5 | 28.5 |
| 5.5 | 5.7 | 29.0 |
| 5.7 | 5.8 | 29.5 |
| 5.8 | 6.0 | 30.0 |
| 6.0 | 6.2 | 30.5 |
| 6.2 | 6.3 | 31.0 |
| 6.3 | 6.5 | 31.5 |
| 6.5 | 6.7 | 32.0 |
| 6.7 | 6.8 | 32.5 |
| 6.8 | 7.0 | 33.0 |

| | | |
|------|------|------|
| 7.0 | 7.2 | 33.5 |
| 7.2 | 7.3 | 34.0 |
| 7.3 | 7.5 | 34.5 |
| 7.5 | 7.7 | 35.0 |
| 7.7 | 7.8 | 35.5 |
| 7.8 | 8.0 | 36.0 |
| 8.0 | 8.2 | 36.5 |
| 8.2 | 8.3 | 37.0 |
| 8.3 | 8.5 | 37.5 |
| 8.5 | 8.7 | 38.0 |
| 8.7 | 8.8 | 38.5 |
| 8.8 | 9.0 | 39.0 |
| 9.0 | 9.2 | 39.5 |
| 9.2 | 9.3 | 40.0 |
| 9.3 | 9.5 | 40.5 |
| 9.5 | 9.7 | 41.0 |
| 9.7 | 9.8 | 41.5 |
| 9.8 | 10.0 | 42.0 |
| 10.0 | | 42.5 |

‘SCHEDULE 7

‘MASS LIMITS FOR NON-CONFORMING VEHICLES WITH SINGLE AXLES OR AXLE GROUPS

schedule 4, section 6

‘Mass limits for single axles and axle groups

‘1. The mass on a single axle or axle group must not be more than the limit for the axle or axle group in schedule 8.

‘Mass limits for vehicles and vehicle combinations

‘2.(1) The loaded mass of a vehicle or combination must not be more than the mass in schedule 9.

‘(2) The total mass of a single vehicle must not be more than the vehicle’s GVM.

‘(3) The total mass of a combination and a load on it must not be more than the towing vehicle’s GCM.

‘(4) The sum of the mass on the axle groups and single axles on a single vehicle or combination must not be more than the sum of the mass limits in schedule 8 for the axle groups and single axles.

‘SCHEDULE 8

‘MASS LIMITS FOR NONCONFORMING SINGLE AXLES AND AXLE GROUPS

schedule 7, section 2

Column 1

Column 2

Axle configuration

Axle mass

(tonnes)

Single axles

- 1. Single axle fitted with 2 tyres 4.6
- 2. Single axle fitted with dual tyres or 4 or more tyres (however arranged) 8.1

Axle groups without retractable axles

- 3. A group of 2 axles with a load sharing system, both axles fitted with dual tyres or 4 or more tyres (however arranged), and a distance between axles of more than 2.4 m 15.0

| | | |
|---|---|------|
| 4. | A group of 2 axles with a load sharing system, both axles fitted with dual tyres or 4 or more tyres (however arranged), and a distance between axles of not more than 2.4 m | 13.2 |
| 5. | A group of 2 axles with a load sharing system, 1 axle fitted with dual tyres, the other axle with single tyres, and a distance between axles of more than 2.4 m | 12.0 |
| 6. | A group of 2 axles with a load sharing system, 1 axle fitted with dual tyres, the other axle with single tyres, and a distance between axles of not more than 2.4 m | 11.2 |
| 7. | Tandem axle group with both axles fitted with single tyres | 9.0 |
| 8. | Twin steer axle group with both axles fitted with single tyres | 9.0 |
| 9. | Triaxle group with each axle fitted with dual tyres or 4 or more tyres (however arranged) | 18.0 |
| 10. | Triaxle group with 2 axles fitted with dual tyres and the other axle with single tyres | 17.5 |
| 11. | Triaxle group with 2 axles fitted with single tyres and the other axle with dual tyres | 15.0 |
| 12. | Triaxle group with each axle fitted with single tyres | 13.8 |
| 13. | Quadaxle group or a group of more than 4 axles with each axle fitted with dual tyres or 4 or more tyres (however arranged) | 18.0 |
| Axle groups with retractable axles | | |
| 14. | Tandem axle group with 1 axle a retractable axle and the other axle is fitted with— | |
| | (a) dual tyres | 8.1 |
| | (b) single tyres | 4.6 |
| 15. | Triaxle group with 1 axle a retractable axle and the other axles are fitted with— | |
| | (a) dual tyres | 13.2 |
| | (b) single tyres | 9.0 |
| | (c) dual tyres on 1 axle and single tyres on the other axle | 11.2 |

‘SCHEDULE 9**‘NON-CONFORMING VEHICLE LOADED MASS**

schedule 7, section 2

| Column 1 | | Column 2 | | | | |
|----------------------------------|----------------------|-----------------------------|----------------|----------------|----------------|----------------|
| Distance extreme (metres) | between axles | Loaded mass (tonnes) | | | | |
| from | to less than | 2 axles | 3 axles | 4 axles | 5 axles | 6 axles |
| 3.0 | 3.3 | 16.4 | 18.6 | .. | .. | .. |
| 3.3 | 3.6 | 16.7 | 19.0 | 21.3 | .. | .. |
| 3.6 | 3.9 | .. | 19.3 | 21.6 | .. | .. |
| 3.9 | 4.2 | .. | 19.7 | 21.9 | .. | .. |
| 4.2 | 4.5 | .. | 20.0 | 22.2 | .. | .. |
| 4.5 | 4.8 | .. | 20.3 | 22.5 | 24.9 | 24.9 |
| 4.8 | 5.1 | .. | 20.6 | 22.8 | 25.1 | 25.1 |
| 5.1 | 5.4 | .. | 21.0 | 23.1 | 25.4 | 25.4 |
| 5.4 | 5.7 | .. | 21.3 | 23.4 | 25.7 | 25.7 |
| 5.7 | 6.0 | .. | 21.6 | 23.7 | 26.0 | 26.0 |
| 6.0 | 6.3 | .. | 21.9 | 24.0 | 26.3 | 26.3 |
| 6.3 | 6.6 | .. | 22.3 | 24.3 | 26.6 | 26.6 |
| 6.6 | 6.9 | .. | 22.6 | 24.6 | 26.8 | 26.8 |
| 6.9 | 7.2 | .. | 23.0 | 24.9 | 27.1 | 27.1 |
| 7.2 | 7.5 | .. | 23.3 | 25.2 | 27.4 | 27.4 |

| | | | | | | |
|------|------|----|------|------|------|------|
| 7.5 | 7.8 | .. | 23.6 | 25.6 | 27.7 | 27.7 |
| 7.8 | 8.1 | .. | .. | 25.9 | 27.9 | 27.9 |
| 8.1 | 8.4 | .. | .. | 26.2 | 28.2 | 28.2 |
| 8.4 | 8.7 | .. | .. | 26.5 | 28.5 | 28.5 |
| 8.7 | 9.0 | .. | .. | 26.8 | 28.8 | 28.8 |
| 9.0 | 9.3 | .. | .. | 27.1 | 29.1 | 31.5 |
| 9.3 | 9.6 | .. | .. | 27.4 | 29.4 | 31.8 |
| 9.6 | 9.9 | .. | .. | 27.7 | 29.6 | 32.1 |
| 9.9 | 10.2 | .. | .. | 28.0 | 29.9 | 32.2 |
| 10.2 | 10.5 | .. | .. | 28.3 | 30.2 | 32.6 |
| 10.5 | 10.8 | .. | .. | 28.6 | 30.5 | 32.9 |
| 10.8 | 11.1 | .. | .. | 28.9 | 30.7 | 33.1 |
| 11.1 | 11.4 | .. | .. | 29.2 | 31.0 | 33.4 |
| 11.4 | 11.7 | .. | .. | 29.5 | 31.3 | 33.7 |
| 11.7 | 12.0 | .. | .. | 29.8 | 31.6 | 33.9 |
| 12.0 | 12.3 | .. | .. | 30.1 | 31.9 | 34.2 |
| 12.3 | 12.6 | .. | .. | 30.4 | 32.2 | 34.5 |
| 12.6 | 12.9 | .. | .. | 30.7 | 32.4 | 34.7 |
| 12.9 | 13.2 | .. | .. | .. | 32.7 | 35.1 |
| 13.2 | 13.5 | .. | .. | .. | 33.0 | 35.3 |
| 13.5 | 13.8 | .. | .. | .. | 33.3 | 35.6 |
| 13.8 | 14.1 | .. | .. | .. | 33.5 | 35.9 |
| 14.1 | 14.4 | .. | .. | .. | 33.8 | 36.0 |
| 14.4 | 14.7 | .. | .. | .. | 34.1 | 36.0 |
| 14.7 | 15.0 | .. | .. | .. | 34.4 | 36.0 |
| 15.0 | 15.3 | .. | .. | .. | 34.7 | 36.0 |
| 15.3 | 15.6 | .. | .. | .. | 35.0 | 36.0 |

| | | | | | | |
|------|------|----|----|----|------|------|
| 15.6 | 15.9 | .. | .. | .. | 35.3 | 36.0 |
| 15.9 | 16.2 | .. | .. | .. | 35.5 | 36.0 |
| 16.2 | 16.5 | .. | .. | .. | 35.8 | 36.0 |

‘SCHEDULE 10

‘LOADING REQUIREMENTS

schedule 11, definition “loading requirement”

‘Loading obligations

‘**1.(1)** A load on a vehicle must not be placed in a way that makes the vehicle unstable or unsafe.

‘**(2)** A load on a vehicle must be secured so it is unlikely to fall or be dislodged from the vehicle.

‘**(3)** An appropriate method must be used to restrain the load on a vehicle.⁴

‘Trailers

‘**2.(1)** A trailer in a combination must be securely coupled to the vehicle in front of it.

‘**(2)** The components of a coupling used between vehicles must be compatible and properly connected to each other.’

Amendment of Sch 5 (Dictionary)

7.(1) Schedule 5—

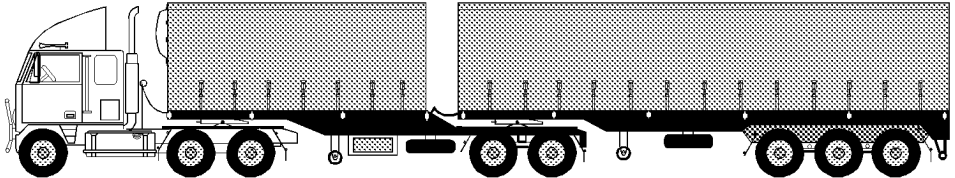
renumber as schedule 11.

⁴ See the Load Restraint Guide for examples of safe ways of loading vehicles.

(2) Schedule 11 (as renumbered)—

insert—

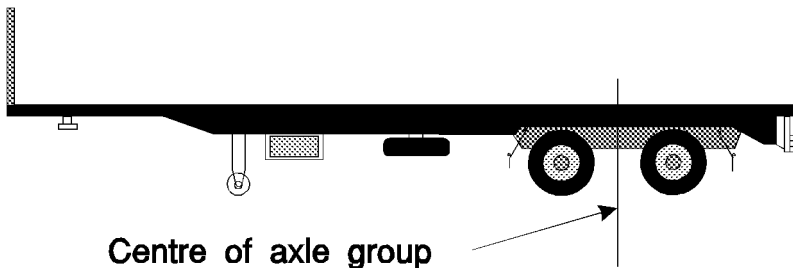
- ‘ **“B-double”**’ means a combination consisting of a prime mover towing 2 semi-trailers.



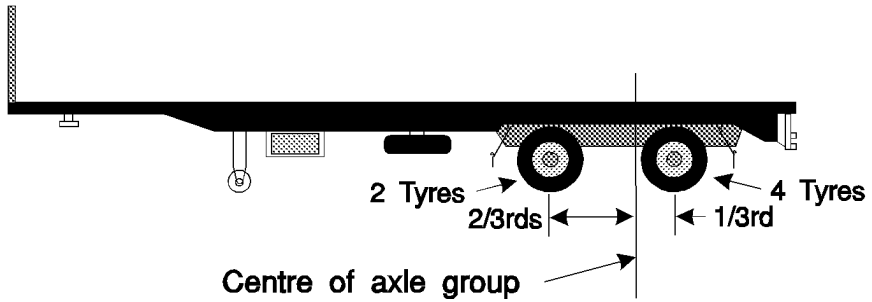
Typical B-double

“centre of an axle group” means—

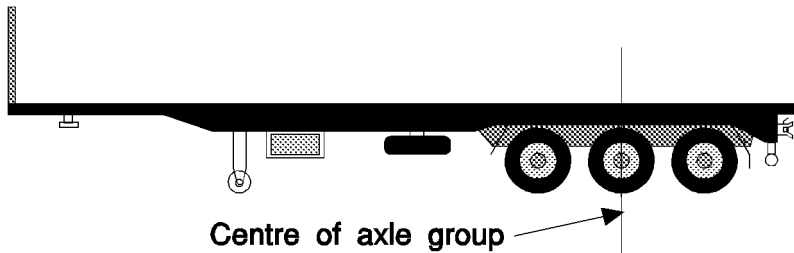
- (a) a line located midway between the centre-lines of the outermost axles of the group; or
- (b) if there are 2 axles in the group and one of them is fitted with twice the number of tyres as the other axle—a line located one third of the way from the centre-line of the axle with more tyres towards the centre-line of the axle with fewer tyres.



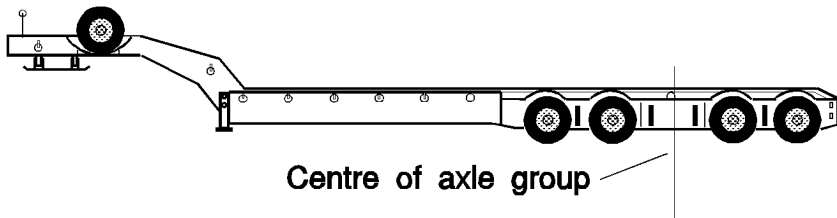
Centre of a typical tandem axle group fitted with an equal number of tyres on each axle



Centre of a typical tandem axle group fitted with a different number of tyres on each axle



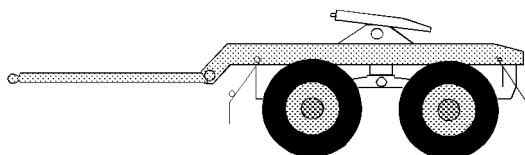
Centre of a typical triaxle group



Centre of a typical quad-axle group

“**combination**” means a motor vehicle connected to one or more trailers.

“**converter dolly**” means a trailer with one axle group or single axle and a fifth wheel coupling designed to convert a semi-trailer into a dog trailer.



Typical converter dolly

“drawbar” means a part of a trailer (other than a semi-trailer) that connects the trailer body to a coupling for towing purposes.

“driver” means the person driving or in control of a motor vehicle.

“fifth wheel coupling” means a device, other than the upper rotating element and the kingpin (which are parts of a semi-trailer), used with a prime mover, semi-trailer or a converter dolly to permit quick coupling and uncoupling and to provide for a point of rotation.

“GCM” (gross combination mass) means the maximum loaded mass of a motor vehicle and any vehicles it may lawfully tow—

- (a) stated by the vehicle’s manufacturer—
 - (i) on the vehicle’s compliance plate; or
 - (ii) if there is no compliance plate—in another place; or
- (b) stated by the vehicle registration authority if—
 - (i) the manufacturer has not stated the vehicle’s maximum loaded mass; or
 - (ii) the manufacturer cannot be identified; or
 - (iii) the vehicle has been modified and the maximum loaded mass stated by the manufacturer is no longer appropriate.

“livestock vehicle” means a single vehicle or a combination built to carry livestock.

“load”, of a vehicle, includes—

- (a) anything normally removed from the vehicle when it is not in use; and
- (b) anything that may fall from the vehicle when it is in motion.

“loading requirement”, for a vehicle, means a provision of schedule 10 applying to it.

“load-sharing suspension” means an axle group suspension system—

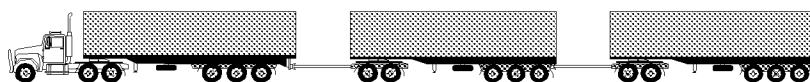
- (a) built to divide the load between the tyres on the group so no tyre carries a mass more than 10% above the mass it would carry if the load were divided equally; and
- (b) with effective damping characteristics on all axles of the group.

“mass requirement”, for a vehicle, means a provision of schedule 4 or 7 applying to it.

“quadaxle group” means a group of 4 axles with a horizontal distance between the centre lines of the outermost axles of more than 3.2 m but not more than 4.9 m.

“retractable axle” means an axle that can be raised so the tyres on it do not touch the ground.

“road train” means a combination (other than a B-double) consisting of a motor vehicle towing at least 2 trailers (counting as one trailer a converter dolly supporting a semi-trailer).



Typical triple road train

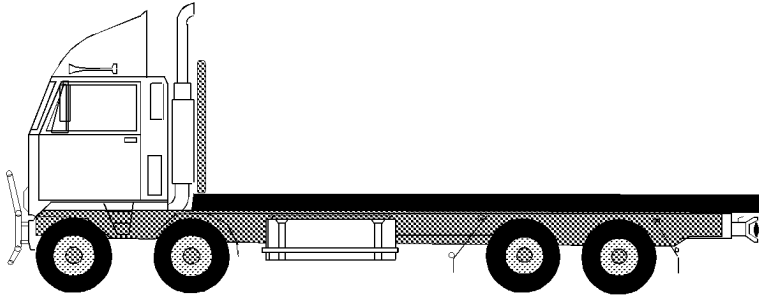
“single axle group” means a group of 2 or more axles with a horizontal distance between the centre-lines of the outermost axles of less than 1 m.

“tag trailer” means a semi-trailer that does not impose a load on the front axle or axle group of the towing vehicle.

“tandem axle group” means a group of at least 2 axles with a horizontal distance between the centre-lines of the outermost axles of at least 1 m but not more than 2 m.

“triaxle group” means a group of at least 3 axles with a horizontal distance between the centre-lines of the outermost axle of more than 2 m but not more than 3.2 m.

“twinsteer axle group” means a group of 2 axles connected to the same steering mechanism on a motor vehicle fitted with single tyres, and with a horizontal distance between the centre-lines of the axles of at least 1 m but not more than 2 m.



Typical twinsteer axle group on a motor vehicle

“**vehicle**” includes anything the vehicle carries that is essential for its operation.’.

Insertion of attachment

8. After schedule 11 (as renumbered)—

insert—

‘ATTACHMENT NOT FORMING PART OF THE REGULATION

‘SECTION 3.05 OF TRANSPORT INFRASTRUCTURE (ROADS) REGULATION 1991 AS IN FORCE IMMEDIATELY BEFORE 1 JULY 1996

‘Transport of livestock

‘3.05(1) The chief executive may issue a permit allowing a vehicle carrying only livestock to exceed the maximum mass permitted under this regulation.

‘(2) An application for a permit must be made to the chief executive in the approved form.

‘(3) A permit may be issued subject to such terms and conditions as the chief executive may determine, including, but not limited to, any of the following terms and conditions—

- (a) the permit must be carried in the motor vehicle whenever the vehicle or vehicle combination is on a road;
- (b) an inspection certificate under the *Motor Vehicles Safety Act 1980* for the vehicle or vehicle combination must be carried in the motor vehicle whenever the vehicle or vehicle combination is on a road;
- (c) livestock is not to be carried on an upper deck unless all lower decks are fully loaded;
- (d) the vehicle must comply with the *State Transport Act 1960* and the *Traffic Act 1949*;
- (e) a motor vehicle fitted at the rear with a tandem axle or a triaxle must be driven on at least 2 axles;
- (f) a semitrailer must be fitted with a dual wheel triaxle group and be hauled by a motor vehicle fitted with a dual wheel tandem drive axle group or dual wheel triaxle drive group;
- (g) a dog trailer must be fitted with a dual wheel tandem axle group at the front and a dual wheel triaxle group at the rear;
- (h) the axle mass of an axle group on a trailer, as evidenced by a certificate from the axle manufacturer or the manufacturer's agent, must be at least—
 - (i) 20 t for a tandem axle group; or
 - (ii) 25 t for a triaxle group;
- (i) the suspension and attachment systems on a trailer, as evidenced by a certificate from the suspension manufacturer or the manufacturer's agent, must be capable of carrying at least—
 - (i) 20 t for a tandem axle group; or
 - (ii) 25 t for a triaxle group;
- (j) each wheel on an axle on a trailer must be fitted with—
 - (i) a drum type brake assembly, capable of developing a torque of at least 13 500 Nm when supplied with air at 550 kPa and fitted with a 125 mm long actuation lever; or

- (ii) a disc type brake assembly, capable of developing a torque of at least 13 500 Nm when supplied with air at 550 kPa;
as evidenced by a certificate from the brake system manufacturer or the manufacturer's agent;
- (k) all tyre mass ratings, as set out in a recognised Tyre and Rim Association manual for a speed of 90 km/h, must be at least the tyre mass set out in column 2 of the following table that corresponds to the axle configuration set out in column 1 of the table—

Table**Tyre mass rating**

| Column 1 | Column 2 |
|---|-------------------------------|
| Axle configuration | Tyre mass (tonnes) |
| 1. Single axle fitted with 2 tyres | 3.0 |
| 2. Twin steer axle group with or without a load sharing system | 2.8 |
| 3. (a) Single axle fitted with dual tyres | 2.5 |
| (b) Tandem axle group with a load sharing system, where both axles are fitted with dual tyres | 2.5 |
| 4. Triaxle group with a load sharing system, where each axle is fitted with dual tyres | 2.1; |

- (l) the manufacturer's GVM rating for a motor vehicle, as evidenced by the compliance plate or by a certificate from the manufacturer or the manufacturer's agent, must be at least the GVM set out in the following table—

Table**GVM rating**

| Front axle | Rear axle | GVM (tonnes) |
|------------------------------------|------------------------------------|-------------------------|
| 1. Single axle fitted with 2 tyres | Single axle fitted with dual tyres | 16 |

| | | | |
|----|--|---|----|
| 2. | Single axle fitted with 2 tyres | Tandem axle group with a load sharing system, where both axles are fitted with dual tyres | 26 |
| 3. | Twin steer axle group with a load sharing system, where both axles are fitted with single tyres | Tandem axle group with a load sharing system, where both axles are fitted with dual tyres | 31 |
| 4. | Twin steer axle group without a load sharing system, where both axles are fitted with single tyres | Tandem axle group with a load sharing system, where both axles are fitted with dual tyres | 30 |
| 5. | Single axle fitted with 2 tyres | Triaxle group with a load sharing system, where each axle is fitted with dual tyres | 31 |
| 6. | Twin steer axle group with a load sharing system, where both axles are fitted with single tyres | Triaxle group with a load sharing system, where each is fitted with dual tyres | 36 |
| 7. | Twin steer axle group without a load sharing system, where both axles are fitted with single tyres | Triaxle group with a load sharing system, where each axle is fitted with dual tyres | 35 |

‘(4) A failure to comply with a term or condition of a permit renders the permit void.’.

PART 3—AMENDMENT OF TRANSPORT INFRASTRUCTURE (ROADS) REGULATION 1991

Regulation amended

9. This part amends the *Transport Infrastructure (Roads) Regulation 1991*.

Omission of ss 3.01–3.05

10. Sections 3.01 to 3.05—
omit.

Amendment of s 3.07 (Checking sites)

11. Section 3.07(1), ‘tare in excess of 4 t’—
omit, insert—
‘GVM of more than 4.5 t’.

Amendment of s 3.08 (Restriction of loads on structures)

12. Section 3.08(2), ‘section 3.04’—
omit, insert—
‘the *Transport Operations (Road Use Management) Regulation 1995*, schedule 4’.

Amendment of s 5.01 (Authorisation by chief executive to use a vehicle that does not comply with the regulation)

13. Section 5.01(1), ‘the provisions’ to ‘the approval.’—
omit, insert—
‘a provision of section 3.08 or 4.01 of this regulation or the *Transport Operations (Road Use Management) Regulation 1995*, schedule 4 stated in the approval.’.

Insertion of new s 5.08

14. After section 5.07—

insert—

‘Transitional provision for section 3.05

‘5.08(1) Section 3.05 continues to apply after the commencement of this section to a vehicle to which it applied before the commencement.

‘(2) This section has effect despite the repeal of section 3.05.

‘(3) This section expires on 1 July 1996.’.

Amendment of sch 1 (Fees)

15. Schedule 1, part 3, item 1(b)—

omit, insert—

‘(b) traditional

297.00 plus manufacturing
costs’.

PART 4—AMENDMENT OF TRAFFIC REGULATION 1962

Regulation amended

16. This part amends the *Traffic Regulation 1962*.

Amendment of s 4 (Definitions)

17. Section 4, definitions “**freight container**” and “**twist lock**”—

omit.

Amendment of s 75A (Portable warning signs for heavy motor vehicles)

18. Section 75A(1), definition “**heavy motor vehicle**”, ‘4 t’—
omit, insert—
‘4.5 t’.

Omission of s 77 (Loading to be securely fastened and parallel to sides)

19. Section 77—
omit.

ENDNOTES

1. Made by the Governor in Council on 21 December 1995.
2. Notified in the gazette on 22 December 1995.
3. Laid before the Legislative Assembly on . . .
4. The administering agency is the Department of Transport.